

THREE ARTS CLUB

340 WEST 85TH STREET
LPC-23-11249
ACCESSIBLE ENTRANCE RAMP AND STAIRS



Three Arts Club 2020.02
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Curtis +
Ginsberg
Architects



ARCHITECT

Curtis + Ginsberg Architects
55 Broad Street, Floor 8
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T: 212-929-4417



HISTORIC PRESERVATION

Mary Kay Judy
Architectural & Cultural Heritage Conservation
123 Bowery, 4th Floor
New York, New York 10002

T: 917-886-1719



STRUCTURAL

Old Structures Engineering
90 Broad St, Suite 1501
New York, New York 1004

T: 212-244-4546



MEPF

Dagher Engineering
29 Broadway
New York, New York 10006

T: 212-480-2591



ENERGY/SUSTAINABILITY

Steven Winter Associates, Inc.
307 Seventh Ave, Suite 1701
New York, New York 10001

T: 212-564-5800



ACCESSIBILITY

United Spinal Association
120-34 Queens Blvd. #320
Kew Gardens, New York 11415

T: 718-803-3782

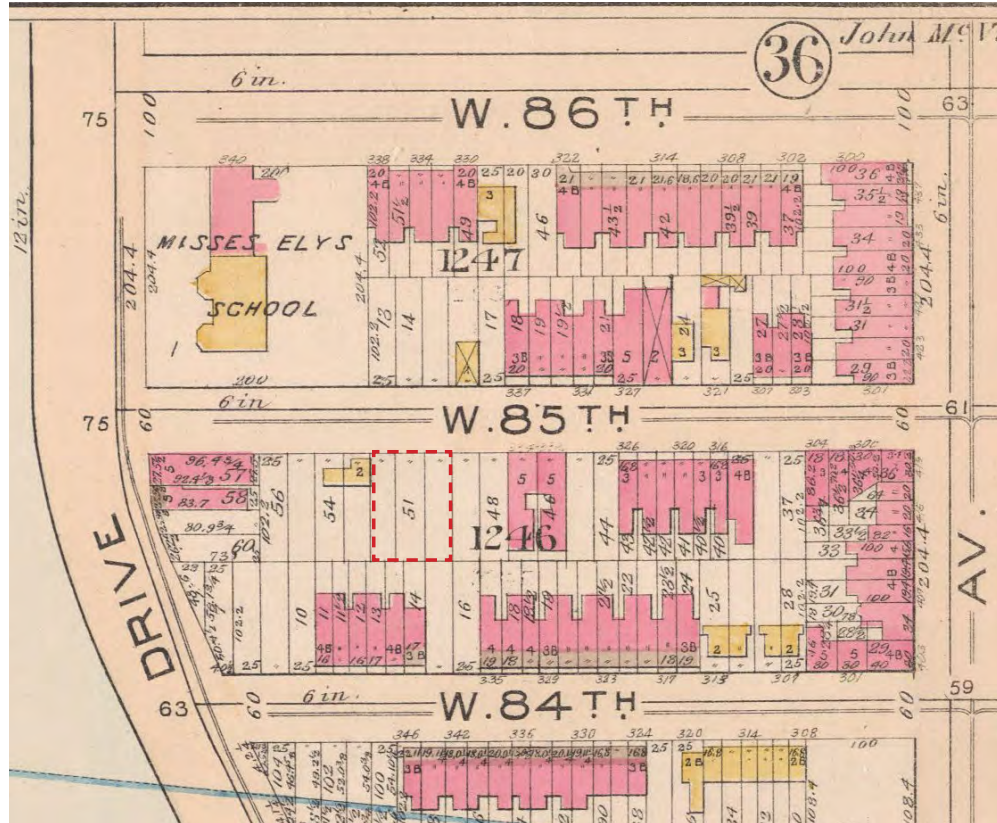


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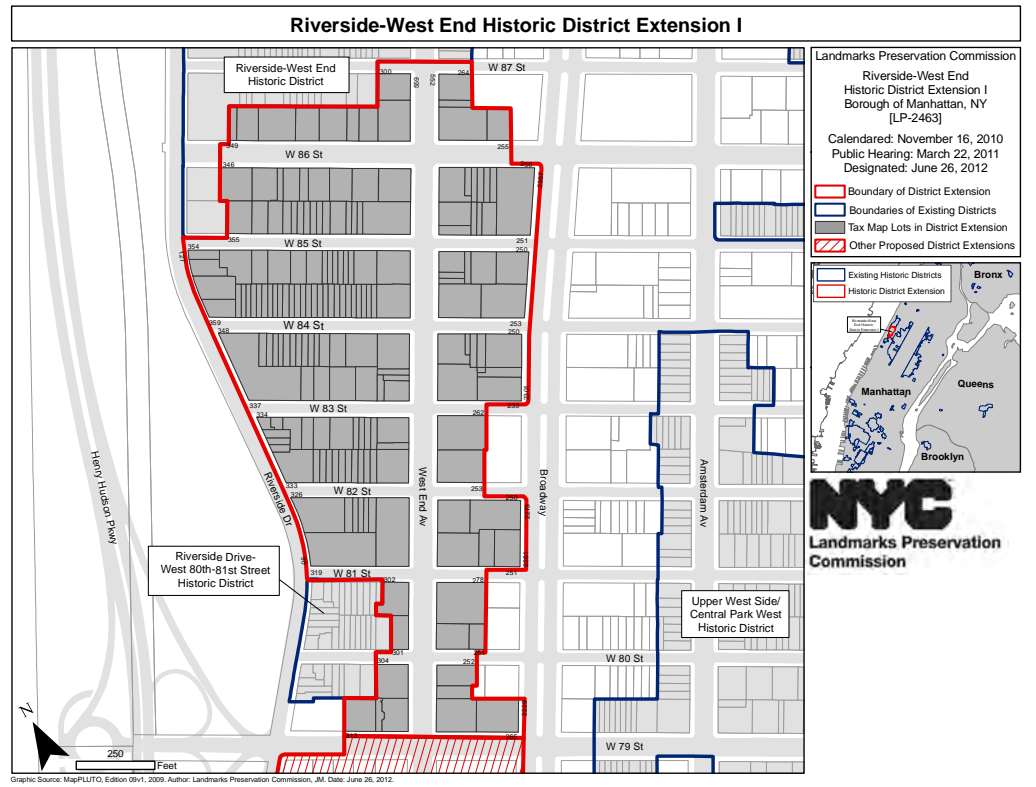




Three Arts Club, 1926-1927
Riverside Drive - West End Historic District Extension I
Architects including George B. deGersdorff



Bromley Map (NYPL, 1897)
84th Street to 86th Street between West End Ave. and
Riverside Drive



Riverside-West End Historic Distric Extension I (LPC, 2012)
79th Street to 87th Street Broadway and Riverside Drive



Manhattan 1940s Tax Photo, 1940s
(NYC Department of Records)



Manhattan 1980s Tax Photo, 1983-1988
(NYC Department of Records)

EXISTING



PROPOSED



SCOPE OF WORK:

- ACCESSIBLE ENTRANCE RAMP AND STAIRS

PROPOSED EXTERIOR ACCESSIBILITY RAMP / STAIR
PARALLEL TO FACADE:

DIFFERENCE OF 3'-10" BETWEEN SIDEWALK AND FIRST FLOOR*
(RESULTS IN 46 LINEAR FEET OF 1:12 RAMP OVERALL).

PROPOSED TWO-RUN RAMP, PARTIALLY EXTERIOR AND PARTIALLY
INTERIOR.

- EXTERIOR RAMP EXTENDS WEST, TOWARD HIGH POINT OF SLOPED SIDEWALK; STAIR EXTENDS EAST TOWARD LOW POINT.
- STAIR & LANDING SETS WIDTH BASED ON REQUIRED 44" WIDE EGRESS, TIGHTEST POINT IS THE COLUMN PROJECTION.
- OVERALL WIDTH LIMITED TO 4'-0" TO MAXIMIZE SIDEWALK WIDTH OF 14'-11" AND FORESTALL REVOCABLE CONSENT/DOT REVIEW.
- PROJECTION INTO SIDEWALK / RIGHT OF WAY IS LESS THAN THE ADJOINING PROPERTY'S AREAWAY COPING & FENCE
- FAÇADE ALTERATIONS: ENTRY DOOR REDUCED HEIGHT; HALF ENGAGED CORINTHIAN COLUMN'S BASES CONCEALED IN NEW LANDING, PROPORTION OF 1:10 REDUCED TO 1:9.

OPTIONS CONSIDERED IN PRE-DESIGN:

- 1) RAMP FROM SIDEWALK AT WEST ALLEYWAY. NO CHANGE TO FAÇADE.
 - X BLOCKS EXISTING / REQUIRED EGRESS PATH FROM BASEMENT
 - O EXTENSIVE STRUCTURAL CHANGE, COMPROMISED BASEMENT CEILING
 - O ELIMINATES LIGHT AND AIR AT BASEMENT APARTMENT
 - O SPLIT ACCESS TO LOBBY
 - O REQUIRES INTERIOR RAMP
 - O SEPARATED WHEELCHAIR ACCESS
 - O IF ROOFED CREATES NEW ZFA
- 2) INTERIOR LIFT IN FORMER VESTIBULE, NO RAMPING
 - X ALTERS FAÇADE—REMOVE STEPS
 - X LONG TERM OPERATIONAL ISSUES
 - O EXTENSIVE STRUCTURAL CHANGE, COMPROMISED BASEMENT CEILING
 - O LESS USEFUL E.G. SHOPPING CARTS, STROLLERS
- 3) INTERIOR RAMP IN PUBLIC/HISTORIC ROOM
 - X ALTERS HISTORIC PUBLIC ROOM
 - X LOSS OF TAX CREDIT/HIGH CONSTRUCTION COST
 - X ALTERS FAÇADE/REMOVES FACADE
 - O ASYMMETRIC ENTRANCE
 - O LOSS OF SHARED COMMUNITY SPACE
 - O EXTENSIVE STRUCTURAL CHANGE, COMPROMISED BASEMENT CEILING
 - O SEPARATED WHEELCHAIR ACCESS
- 4) EXTERIOR LIFT ON SIDEWALK, INTO PUBLIC ROOM, NO RAMPING

- X ALTERS HISTORIC PUBLIC ROOM
- X ALTERS FAÇADE/REMOVES FACADE
- X REVOCABLE CONSENT / DOT REVIEW.
- X LONG TERM OPERATIONAL ISSUES
 - O ASYMMETRIC ENTRANCE
 - O SPLIT ACCESS TO LOBBY
 - O LESS USEFUL E.G. SHOPPING CARTS, STROLLERS
 - O SEPARATED WHEELCHAIR ACCESS
- 5) WHEELCHAIR LIFT FROM SIDEWALK, NO RAMP
 - X ALTERS HISTORIC PUBLIC ROOM
 - X ALTERS FACADE
 - O LESS USEFUL E.G. SHOPPING CARTS, STROLLERS
 - O SEPARATED WHEELCHAIR ACCESS

ALTERNATES FOR PROPOSED PARALLEL TO FAÇADE CONFIGURATION:

- 1) STAIR EXTENDING TO THE NORTH—ON AXIS WITH DOORS
 - X REDUCES SIDEWALK WIDTH TO 4'-1"
 - X REQUIRES REVOCABLE CONSENT / DOT REVIEW
 - O LESS USEFUL E.G. SHOPPING CARTS, STROLLERS
 - O SEPARATED WHEELCHAIR ACCESS
- 2) SWAP STAIR AND RAMP POSITIONS
 - X EXTENDS RAMP LENGTH, REQUIRES SWITCHBACK & LANDING
 - X BLOCKS EXISTING SIDEWALK ELEVATOR
- 3) INTERIOR LIFT OMITTS INTERIOR RAMP (NOT AN EXTERIOR CHANGE)

*SIDEWALK +70.86' FIRST FLOOR +74.68' (EXTERIOR LANDING +72.49')





Building entrance



Front entrance archway and balconet



Front entrance door



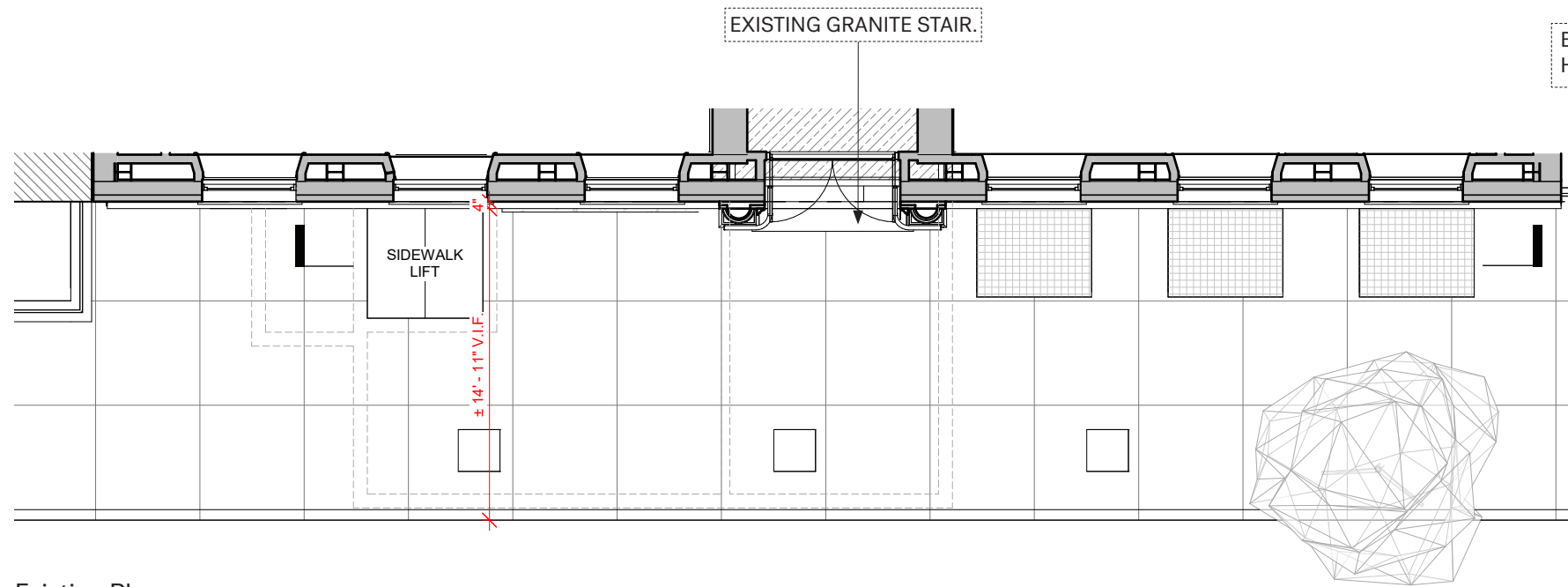
Front entrance archway



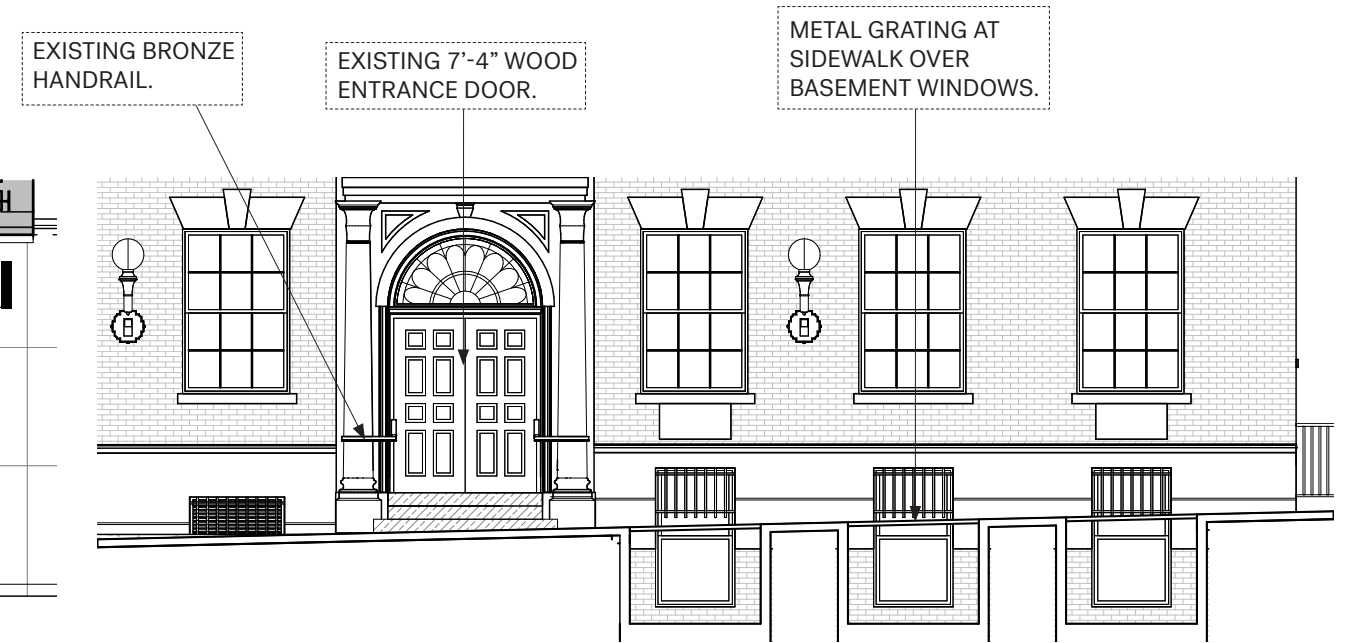
Close up on balconet



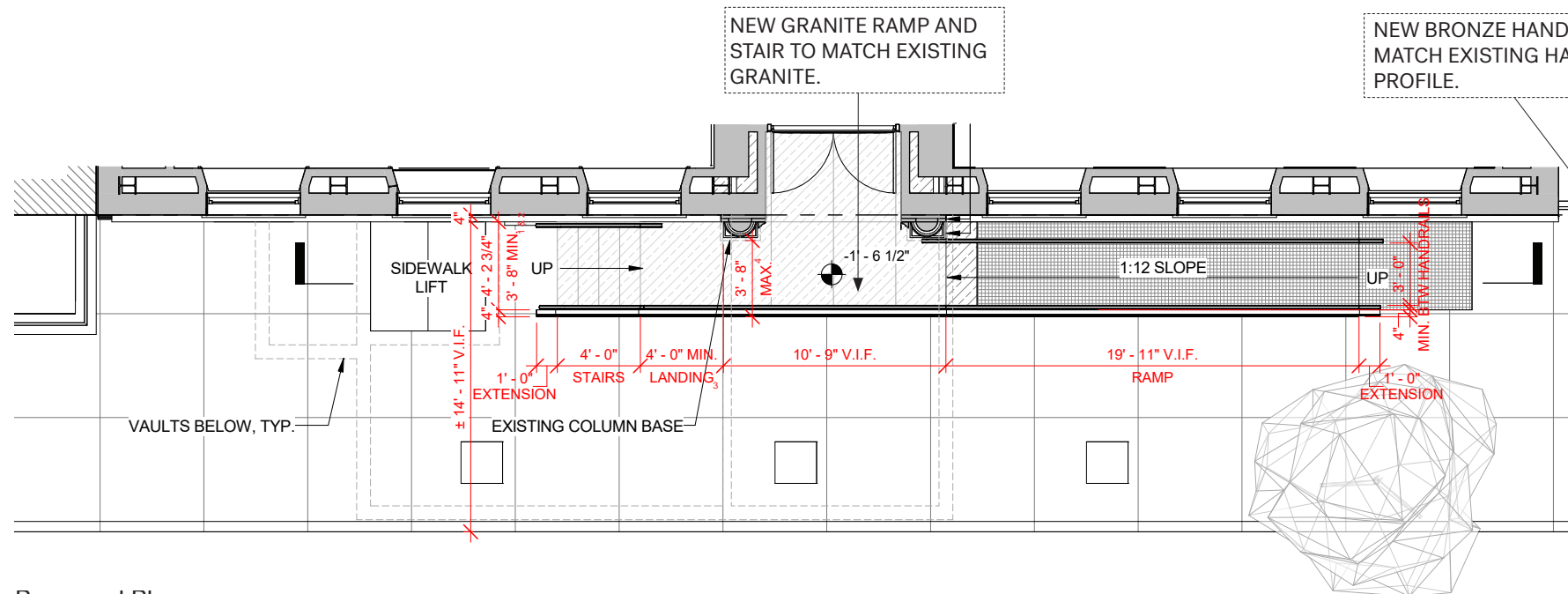
Front entrance steps, column and handrail



Existing Plan



Existing Section



Proposed Plan



Proposed Section

NYC 1968 BUILDING CODE REFERENCES:

- ¹ §27-375 (b) Stairs: 44" WIDE MINIMUM.
- ² §27-375 (d) Stair Landings: WIDTH TO EQUAL REQUIRED STAIR WIDTH

NYC 2022 BUILDING CODE REFERENCES:

- ³ 1011.6 Stairway Landings: 48" DEEP MAXIMUM REQUIRED FOR STRAIGHT RUN STAIRS
- ⁴ 3202.2.2.1 Ramps: 44" MAXIMUM FROM FURTHEST PROTRUDING DECORATIVE ELEMENT
- ⁵ 1015.2 Where Guards are Required: WALKING SURFACES >30" ABOVE SIDEWALK BELOW



LPC-9 | PROPOSED BUILDING ENTRANCE
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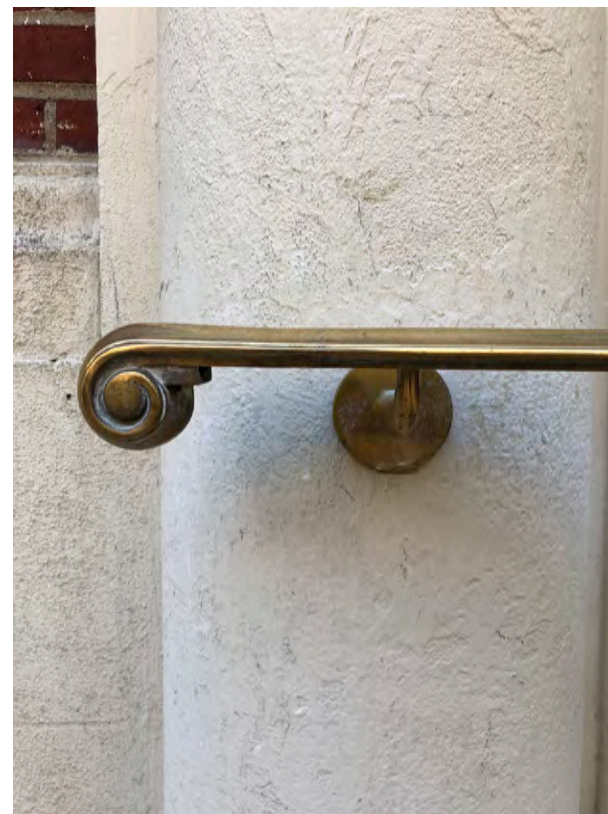




Front entrance steps detail



Front entrance steps, column and handrail



Close up of handrail



Column base detail

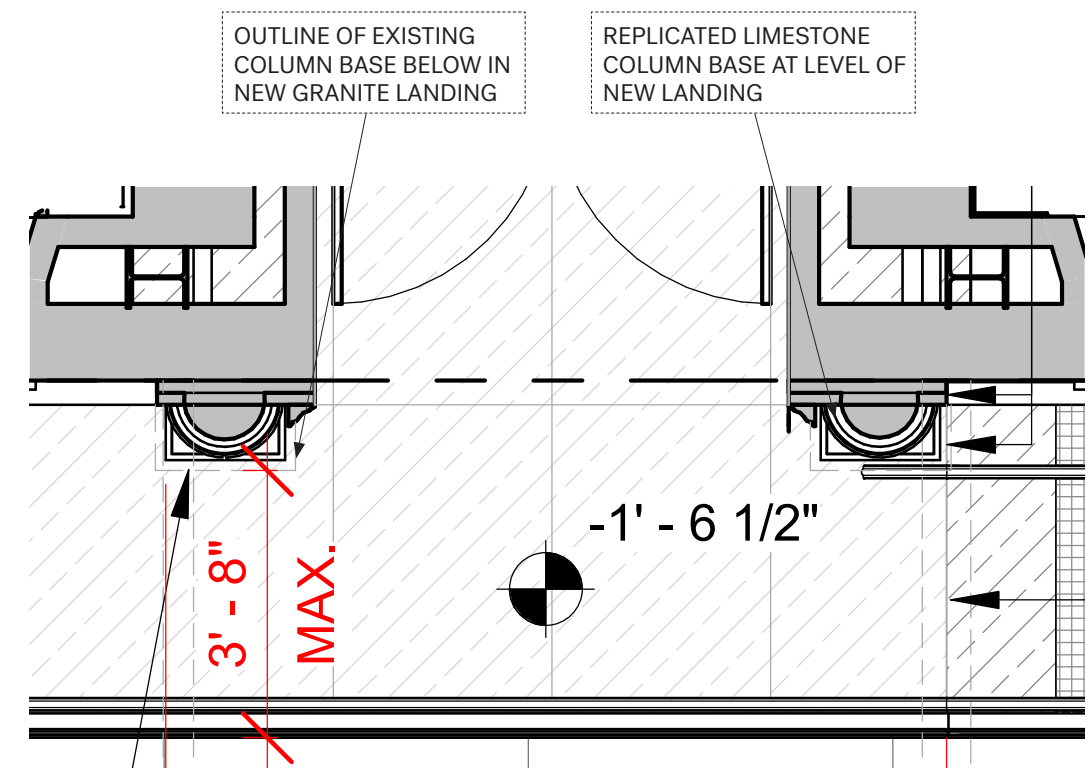
PORTION OF EXISTING COLUMN
BASE TO BE EMBEDDED BELOW
NEW RAMP/STAIR LANDING.



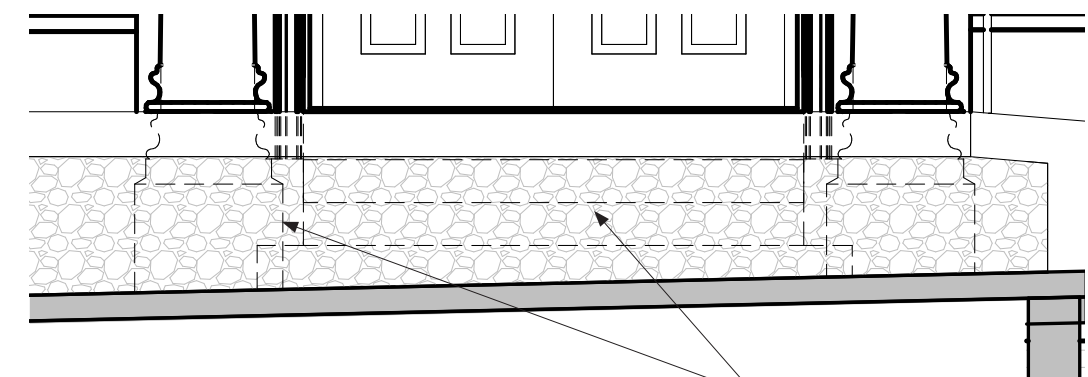
Column base detail



Front entrance steps, column and handrail



Enlarged plan of columns at entry

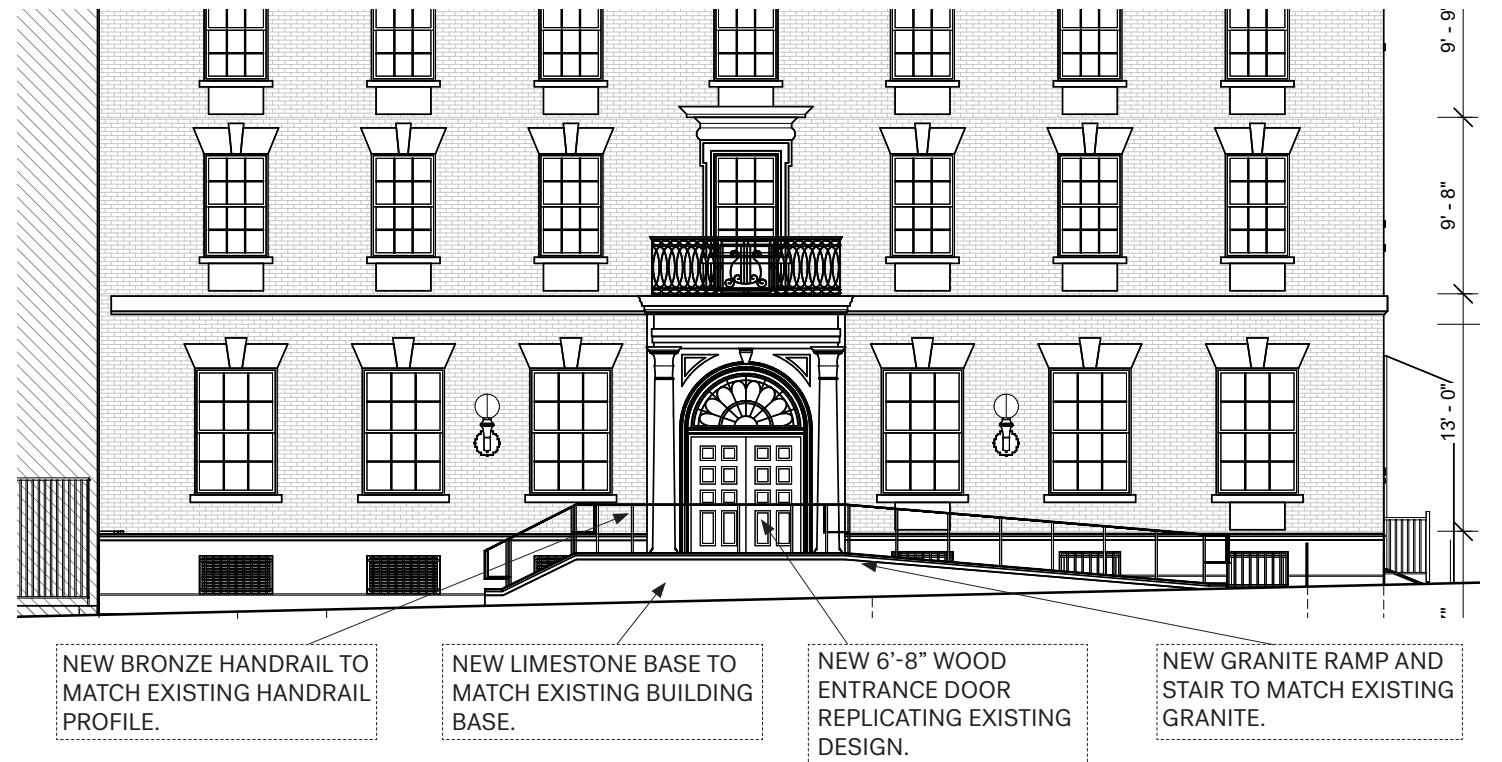


Section of columns at entry

EMBEDDED STAIRS AND
COLUMN BASES



Existing



Proposed



LPC-12 | PROPOSED BUILDING ENTRANCE
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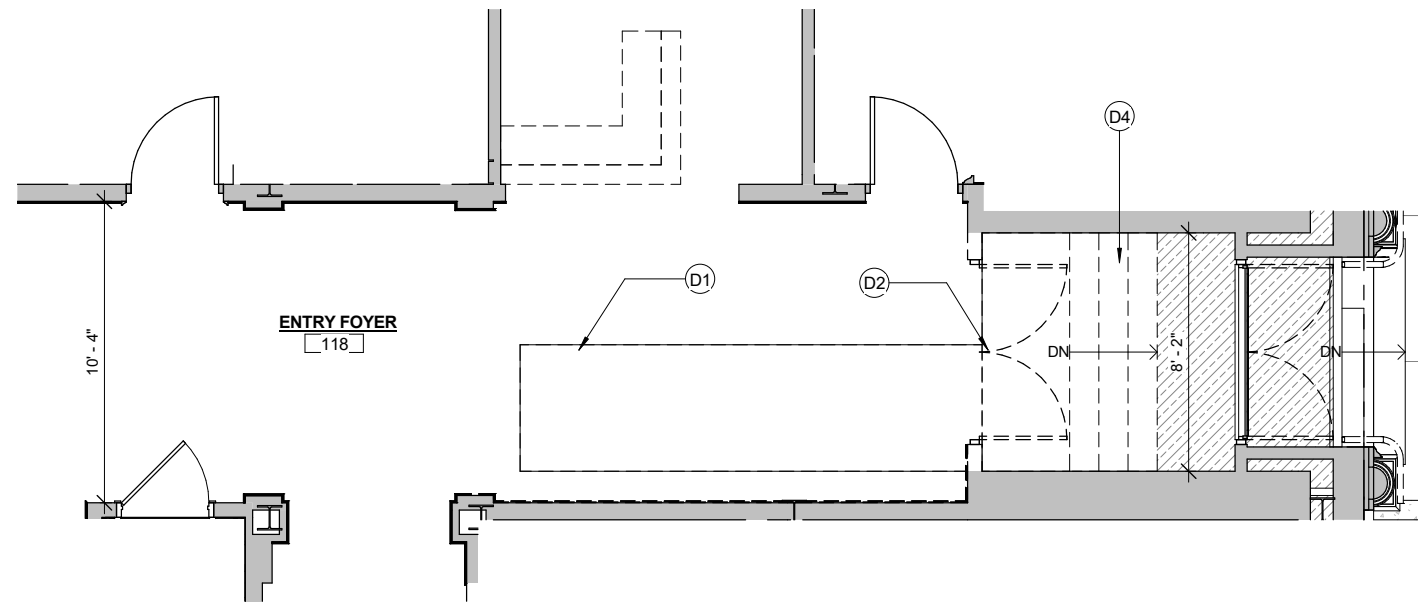




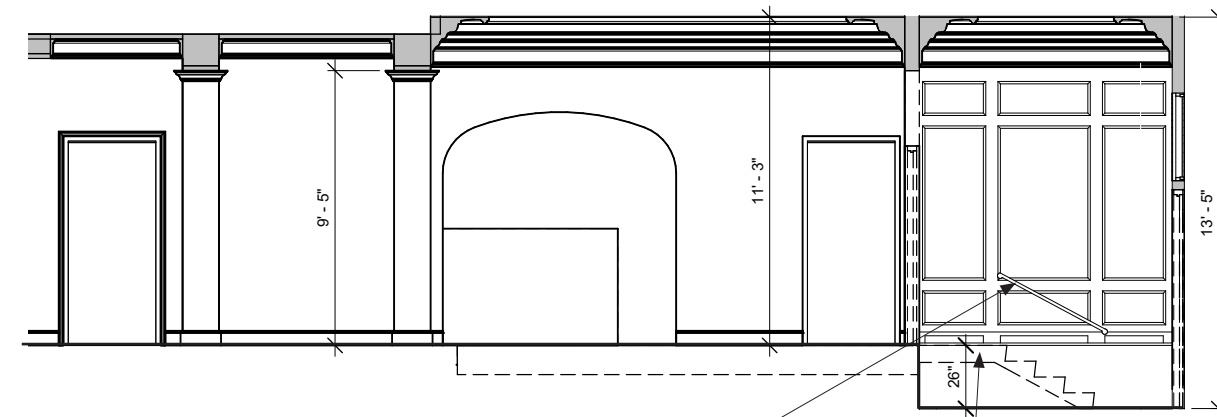
View of ramp, East to West.



View of stair, West to East.

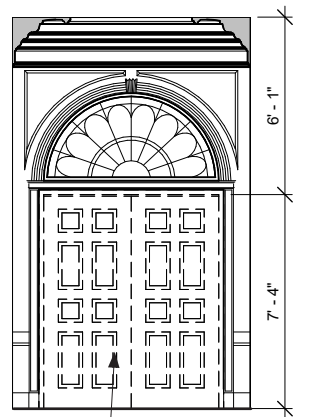


Existing

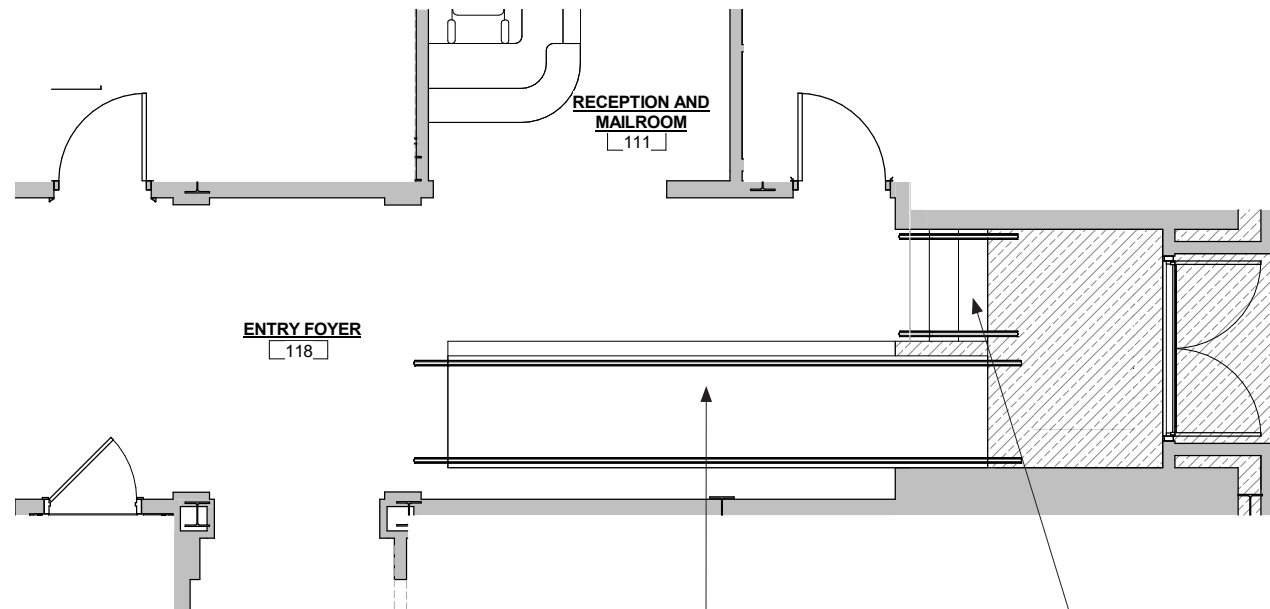


EXISTING BRONZE HANDRAIL

EXISTING GRANITE STAIR



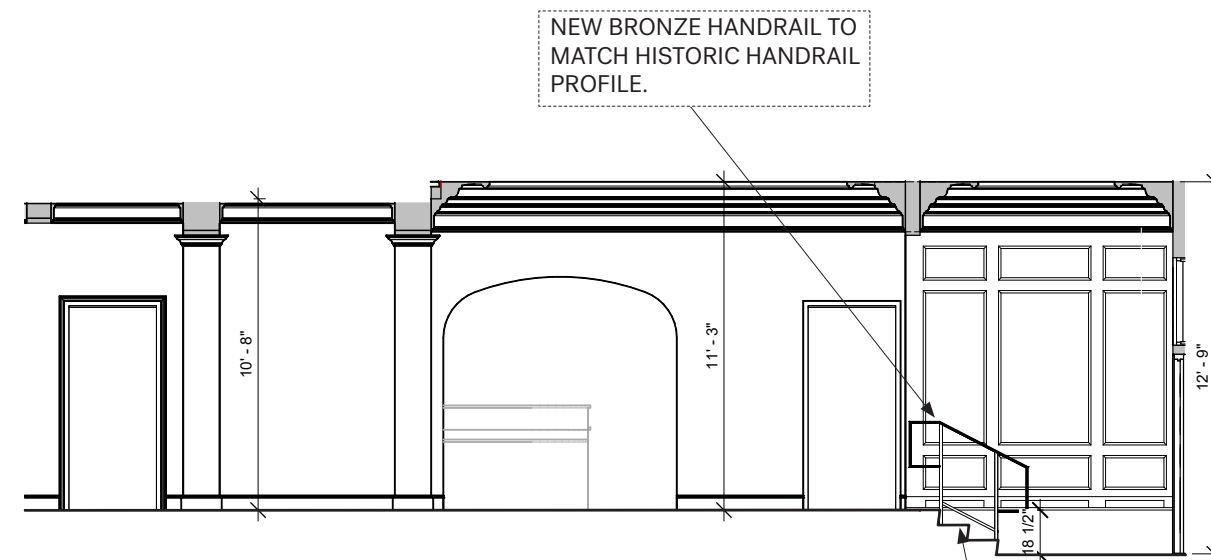
EXISTING 7'-4" WOOD ENTRANCE DOOR



Proposed

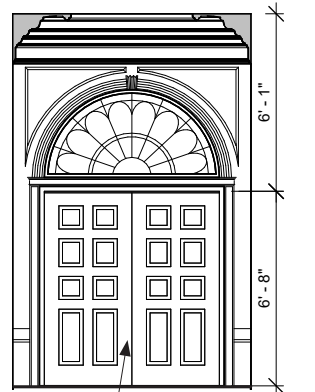
NEW ACCESSIBLE RAMP.

NEW GRANITE STAIR TO MATCH EXISTING GRANITE.

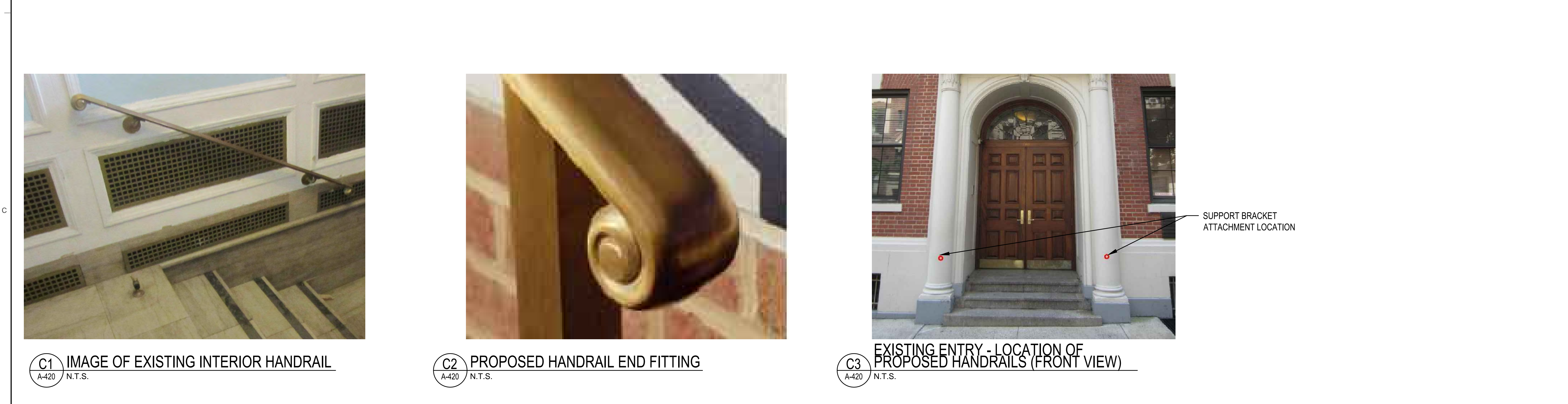
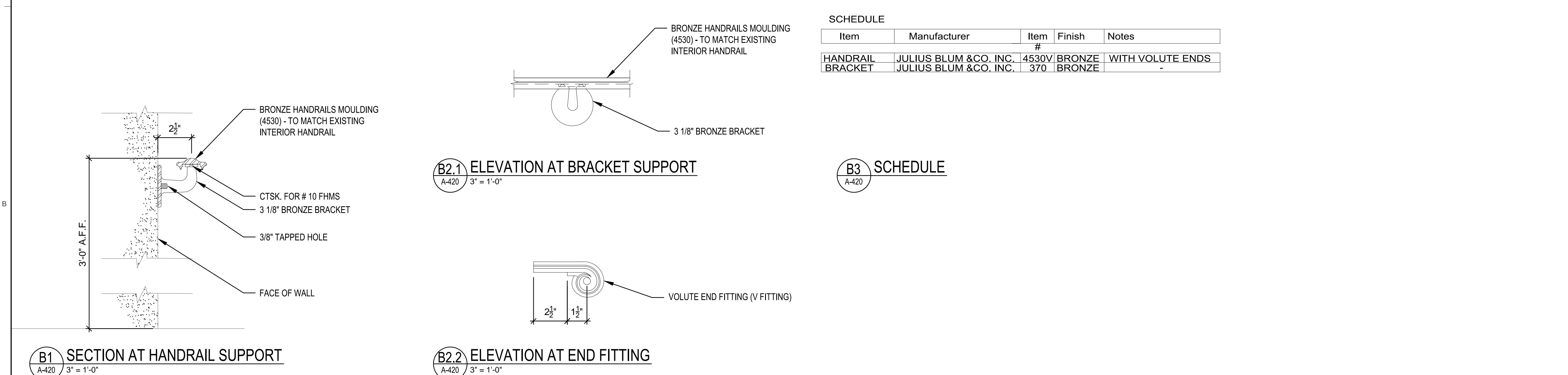
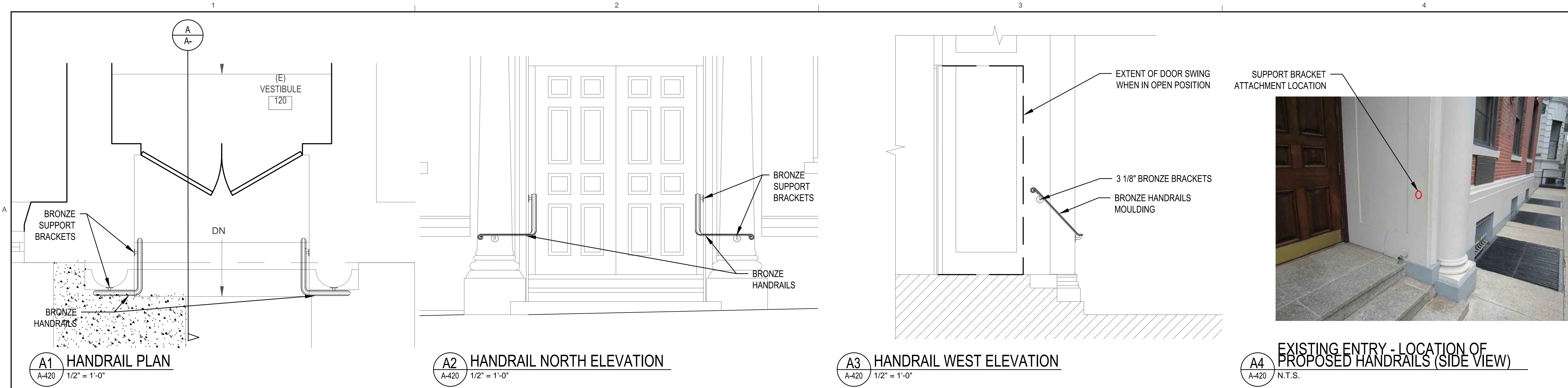


NEW BRONZE HANDRAIL TO MATCH HISTORIC HANDRAIL PROFILE.

NEW GRANITE STAIR TO MATCH EXISTING GRANITE.



NEW 6'-8" WOOD ENTRANCE DOOR REPLICATING EXISTING DESIGN.



340 W 85TH ST

340 W 85TH ST
NEW YORK, NY 10024

Owner
Brandon Housing Development Fund Company, Inc.
2345 Broadway
New York, NY 10024

Architect
CURTIS + GINSBERG ARCHITECTS LLP

299 Broadway, Suite 1107
New York, New York 10007

Structural Engineer
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MEP
Dagher Engineering, PLLC

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New York, NY 10006

Expediting
JM Zoning LLC

299 Broadway, Suite 1100
New York, NY 10007

No.	Date	Revision
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3	07/28/2017	DOB / LPC RESUBMISSION
2	07/27/2017	BID SET
1	06/23/2017	DOB SET

No.	Date	Submission
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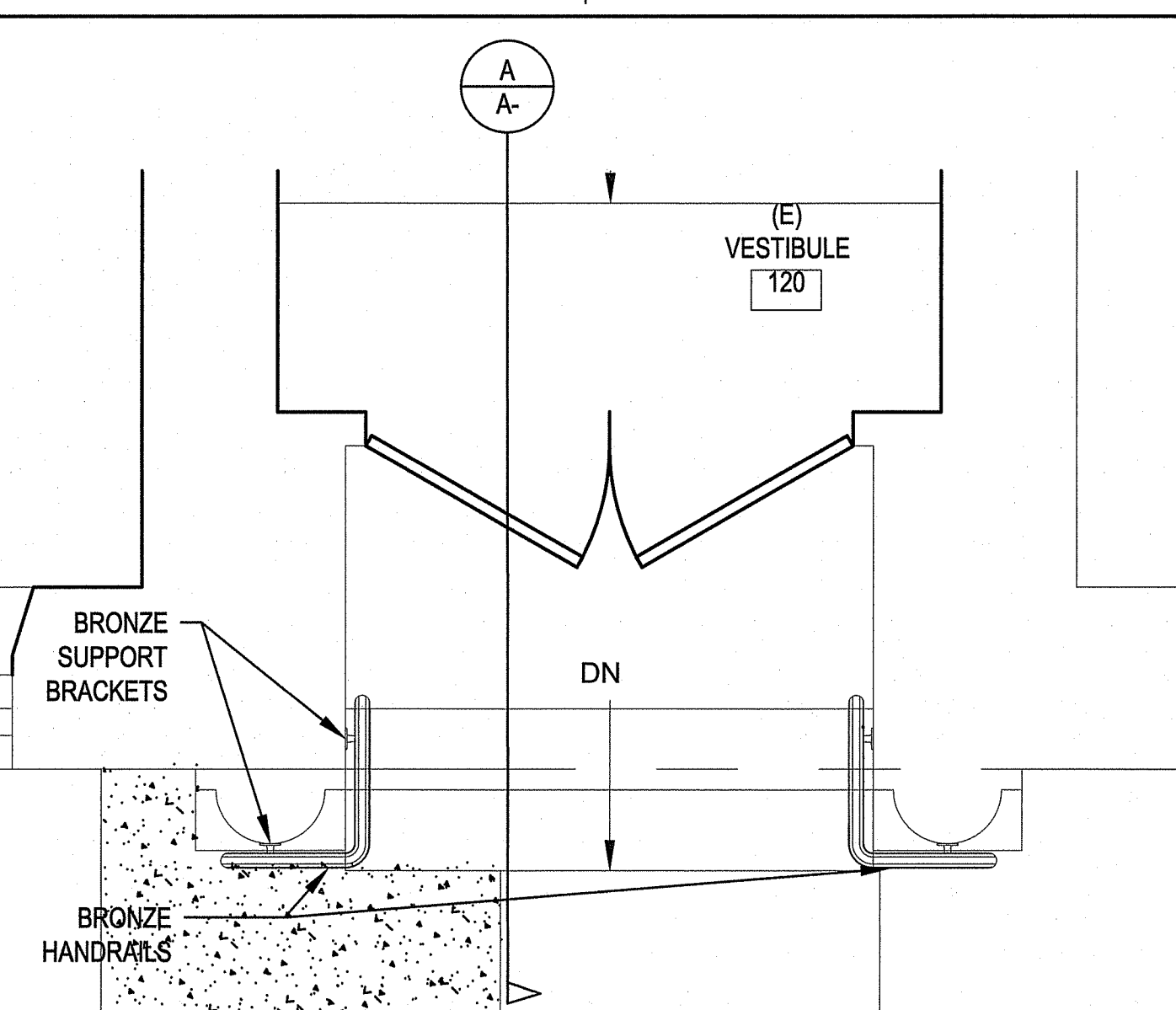
HANDRAIL DETAILS

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Job No.:	1641
Date:	JUNE 23, 2017
Scale:	AS INDICATED
Drawn By:	SO
Checked By:	MEG

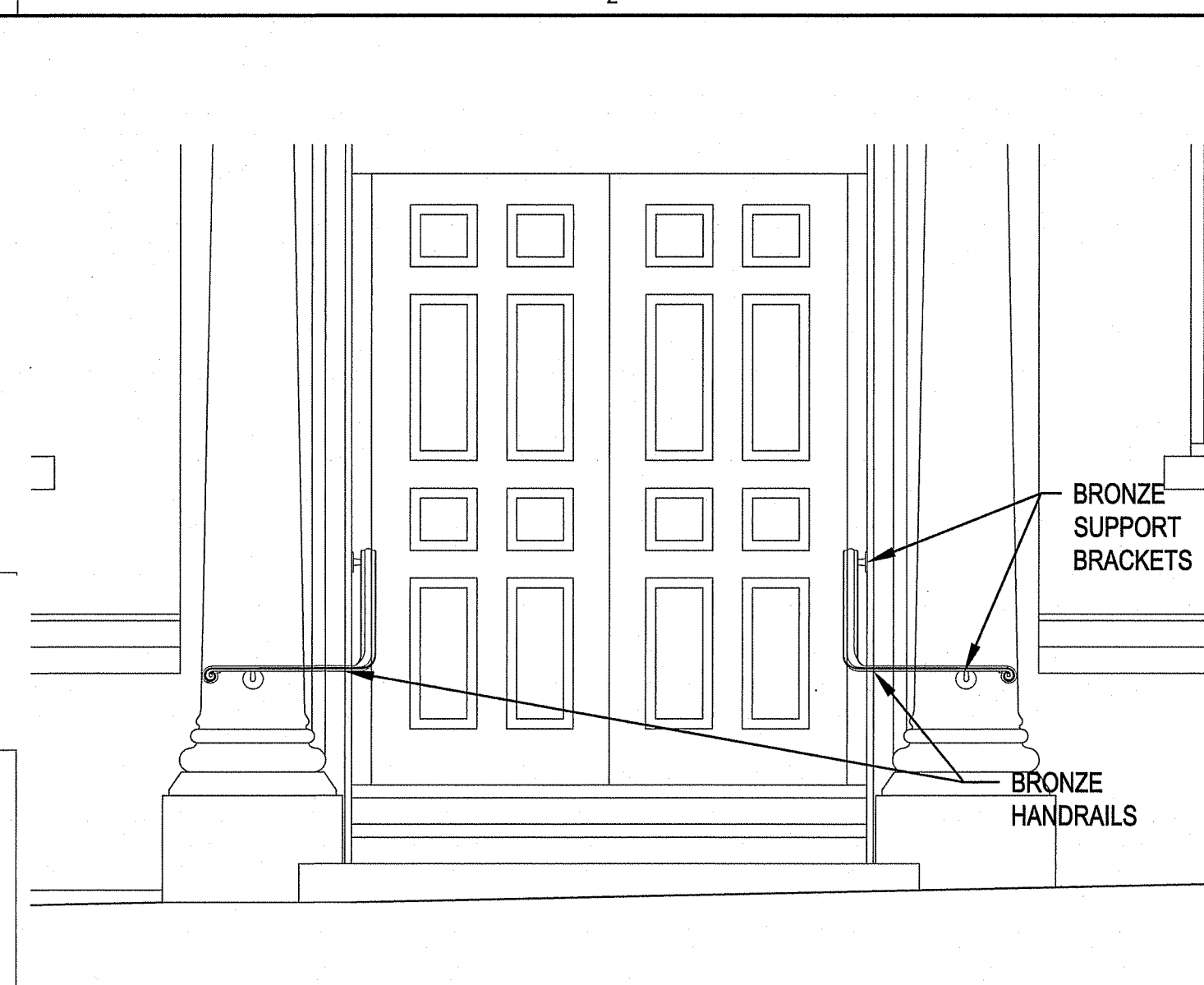
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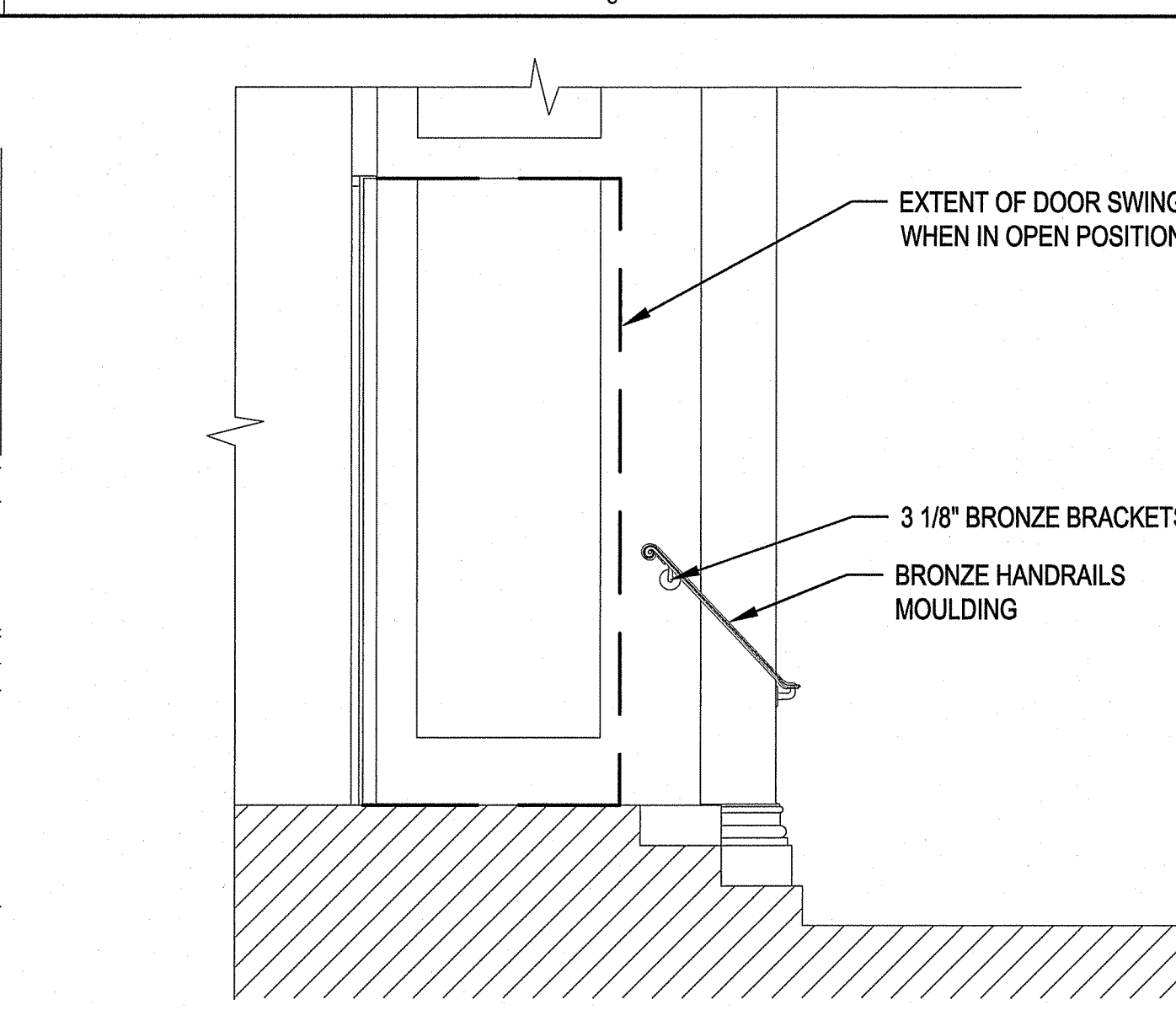
A-420.00



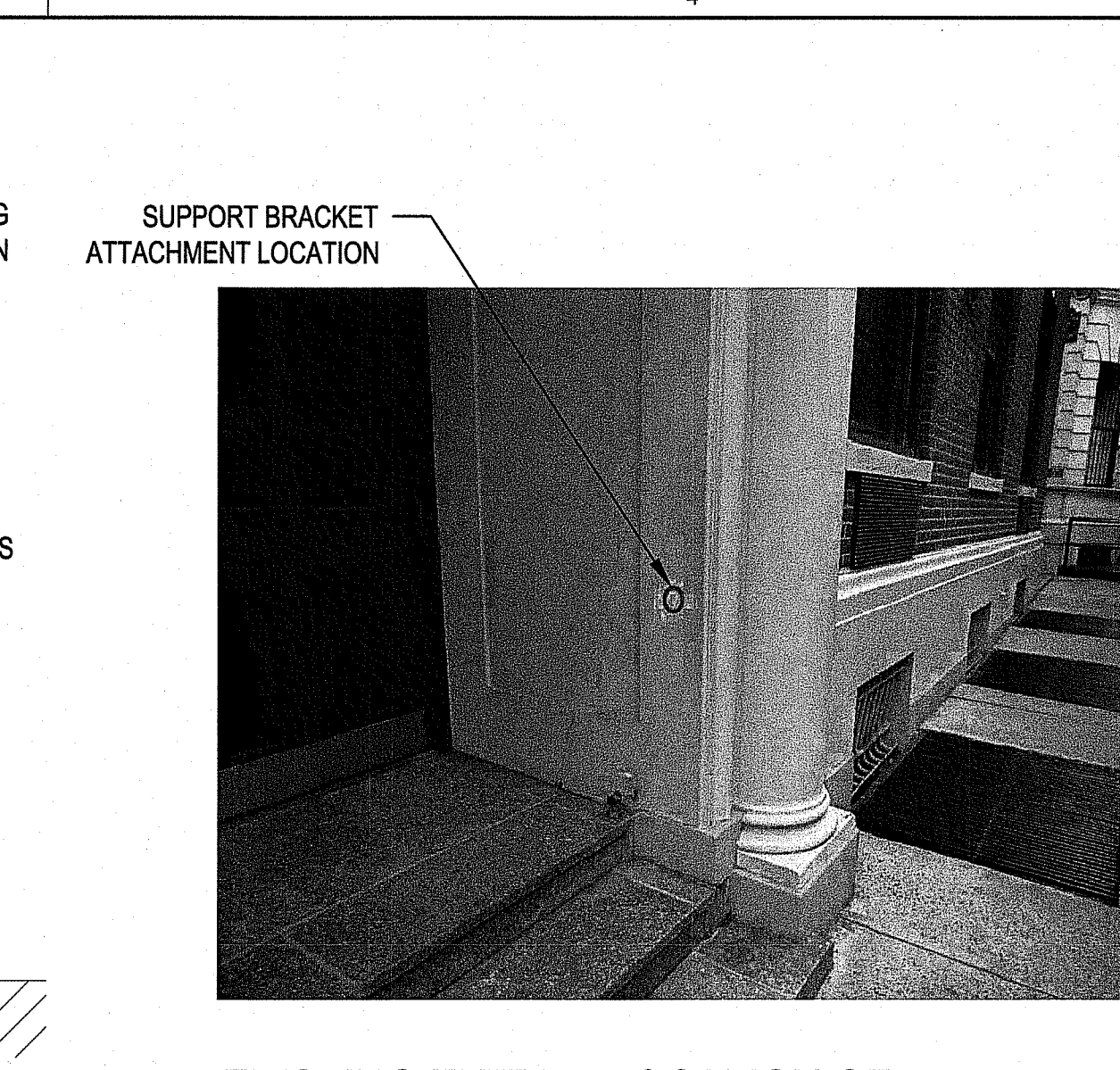
A1 HANDRAIL PLAN
A-420 1/2" = 1'-0"



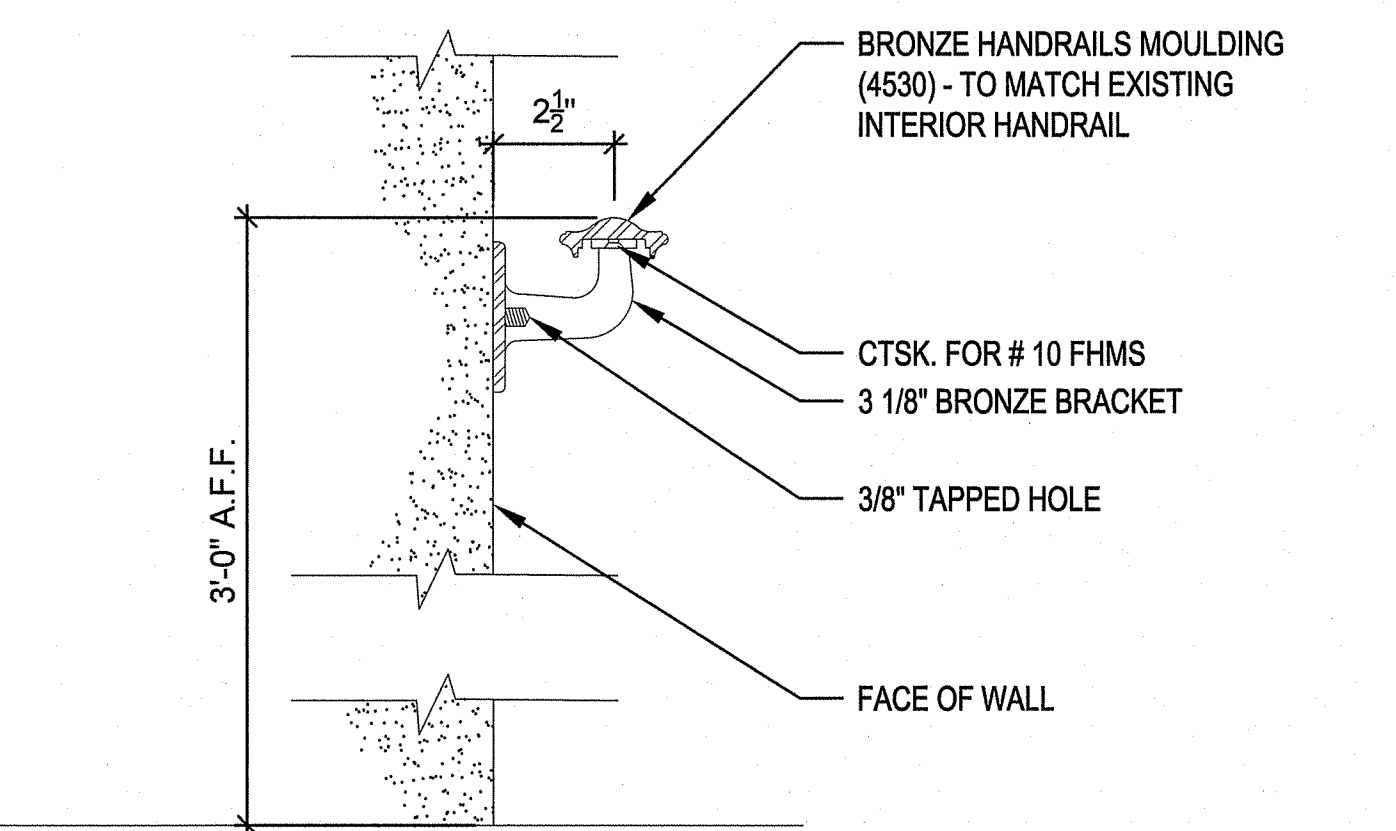
A2 HANDRAIL NORTH ELEVATION
A-420 1/2" = 1'-0"



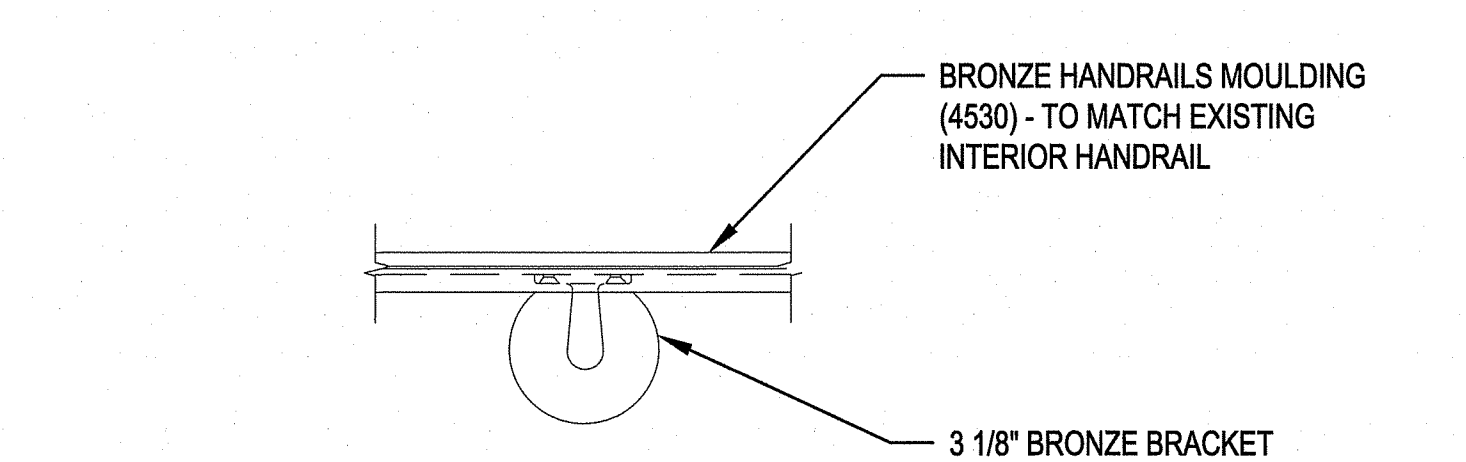
A3 HANDRAIL WEST ELEVATION
A-420 1/2" = 1'-0"



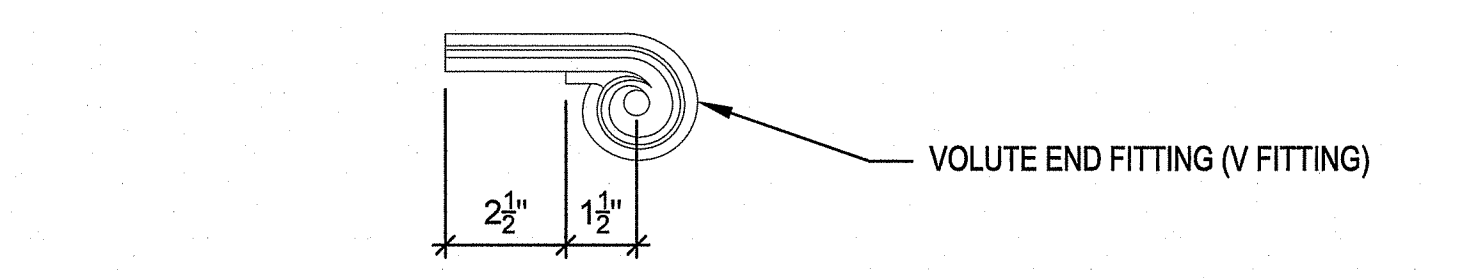
A4 EXISTING ENTRY - LOCATION OF PROPOSED HANDRAILS (SIDE VIEW)
A-420 N.T.S.



B1 SECTION AT HANDRAIL SUPPORT
A-420 3" = 1'-0"



B2.1 ELEVATION AT BRACKET SUPPORT
A-420 3" = 1'-0"

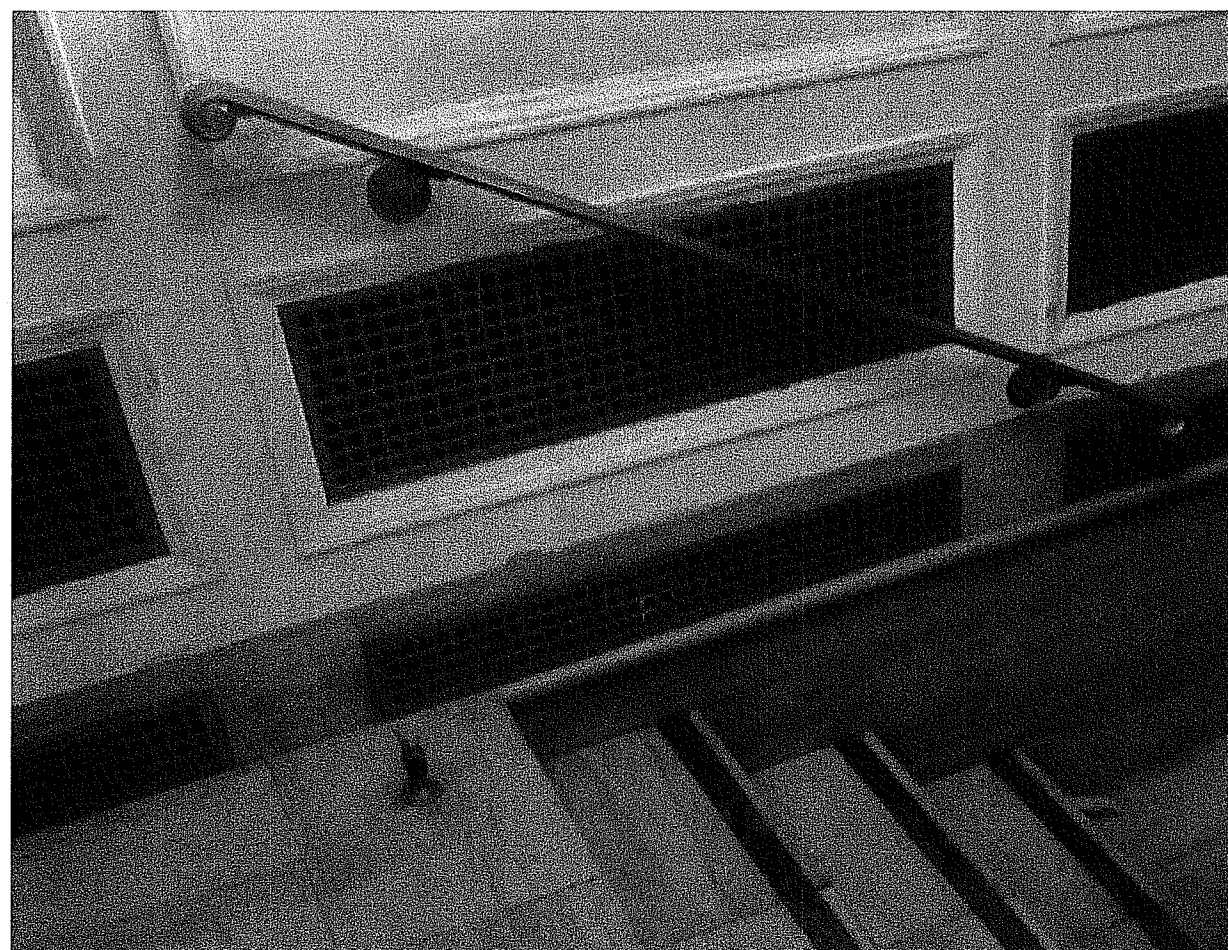


B2.2 ELEVATION AT END FITTING
A-420 3" = 1'-0"

SCHEDULE

Item	Manufacturer	Item #	Finish	Notes
HANDRAIL	JULIUS BLUM & CO. INC.	4530V	BRONZE	WITH VOLUTE ENDS
BRACKET	JULIUS BLUM & CO. INC.	370	BRONZE	-

B3 SCHEDULE
A-420



C1 IMAGE OF EXISTING INTERIOR HANDRAIL
A-420 N.T.S.



C2 PROPOSED HANDRAIL END FITTING
A-420 N.T.S.



C3 EXISTING ENTRY - LOCATION OF PROPOSED HANDRAILS (FRONT VIEW)
A-420 N.T.S.

340 W 85TH ST

340 W 85TH ST
NEW YORK, NY 10024

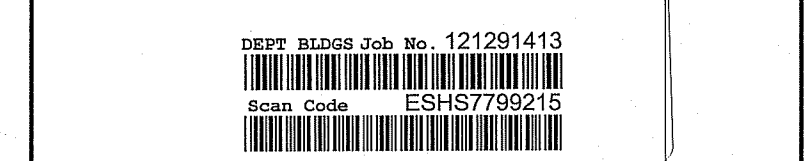
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Expediting
JM Zoning LLC
299 Broadway, Suite 1100
New York, NY 10007

ACCEPTED FOR PERMIT
UNDER DIRECTIVE NO. 14/75
SEP 15 2017
ROFAL KELLINY

No.	Date	Revision
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3	07/28/2017	DOB / LPC RESUBMISSION
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No.	Date	Submission
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Title:
HANDRAIL DETAILS

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A-420.00